



UNION STATION EAST ENHANCEMENTS

GRIDLOCK COSTS OUR ECONOMY



REGIONAL TRANSFORMATION



PLAN



BUILD



OPERATE



GO EXPANSION PROGRAM



INFRASTRUCTURE NEEDS FOR GO EXPANSION

- Bringing more transit options and new levels of service doesn't come easy.
- Metrolinx is undertaking one of the largest infrastructure projects in North America.
- There is community-level disruption required to build a region-wide, integrated transit network.



New Track

150 kilometres of new dedicated GO track will allow for more uninterrupted service.



New Trains

New electric trains will travel faster for longer and reduce travel times.



New Bridges and Tunnels

Bridges and tunnels that eliminate intersections with rail and road traffic will provide more reliable GO train service.



New Renovations

New and improved stations will make your journey more comfortable, from start to finish.



Overhead Catenary

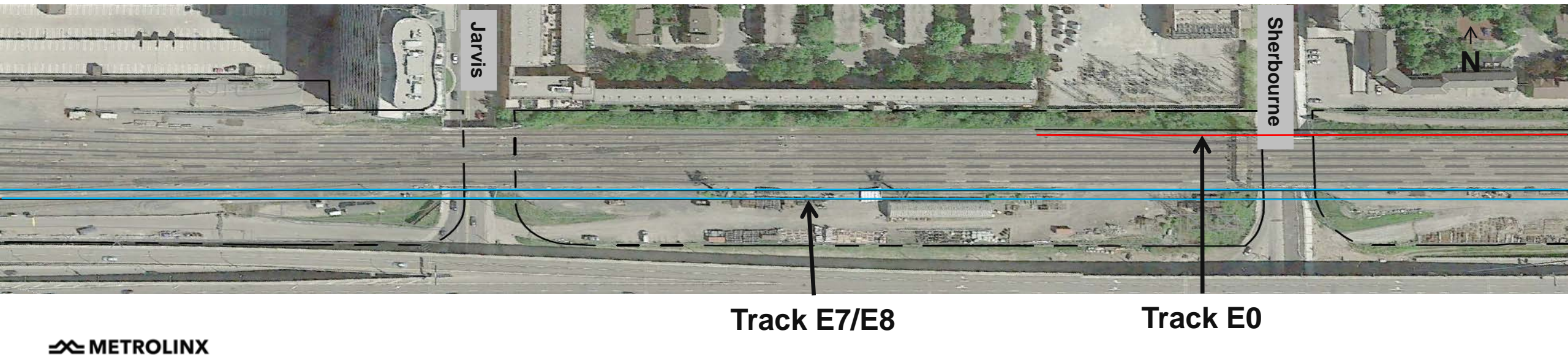
WHAT THIS MEANS FOR YOU

In order to bring more GO for you and your neighbours in the Greater Toronto and Hamilton Area, Metrolinx needs to do the following:

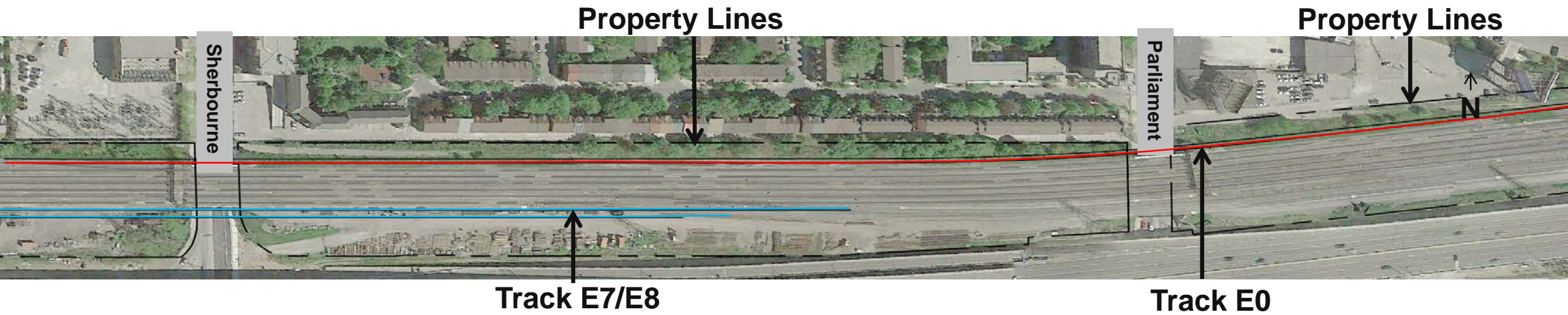
- Add tracks
- Bridge extensions to accommodate new tracks
- Add train storage capacity

ADD TRACK - BETWEEN JARVIS & SHERBOURNE

- Track E0 – to the north; existing track to be extended to the east.
- Tracks E7 & E8 – to the south

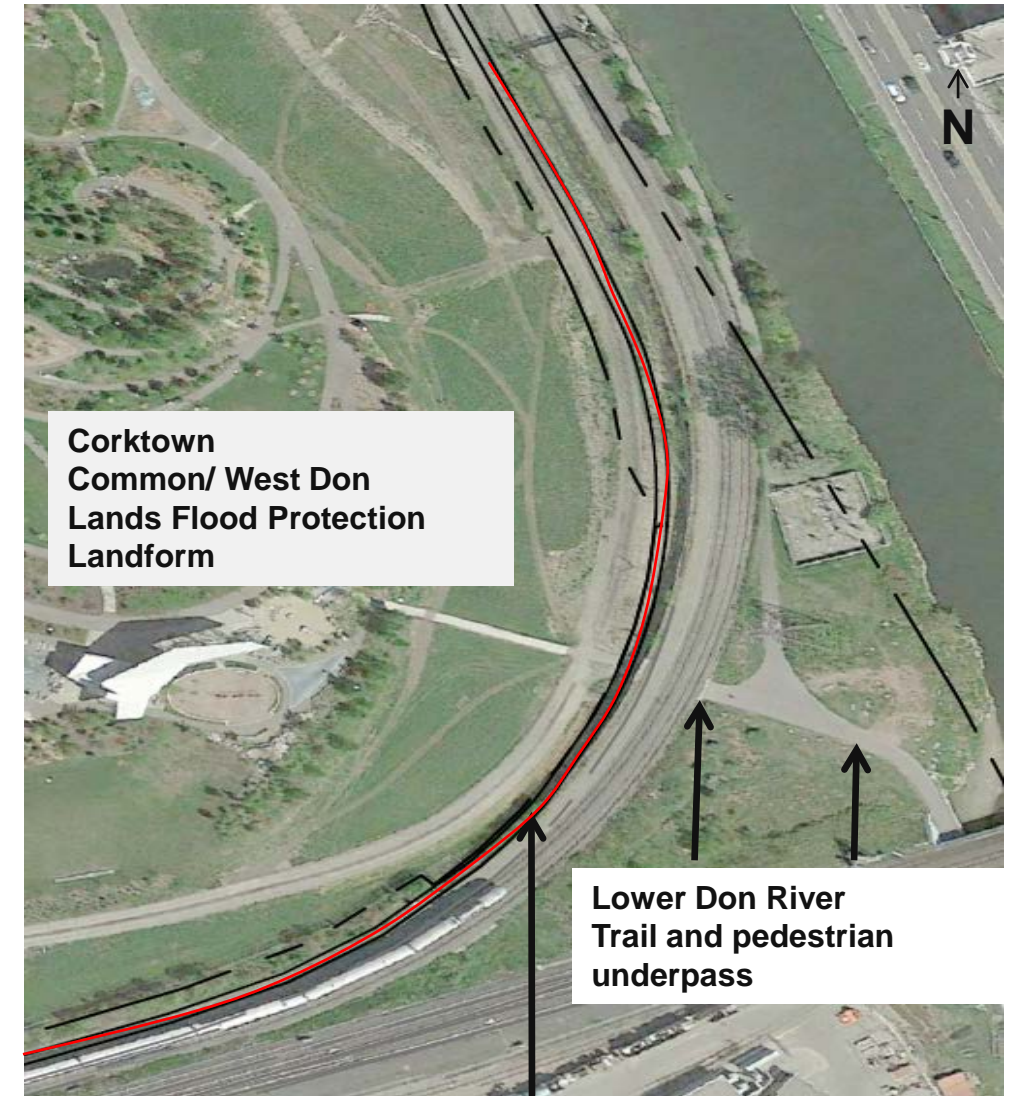


ADD TRACK – BETWEEN SHERBOURNE & DON RIVER



ADD TRACK – CORKTOWN COMMON/WEST DONLANDS

- Majority of this section within the Don River floodplain
- Lower Don River Trail pedestrian underpass will require widening
- Trail disruptions during construction at pedestrian underpass and within Corktown Common



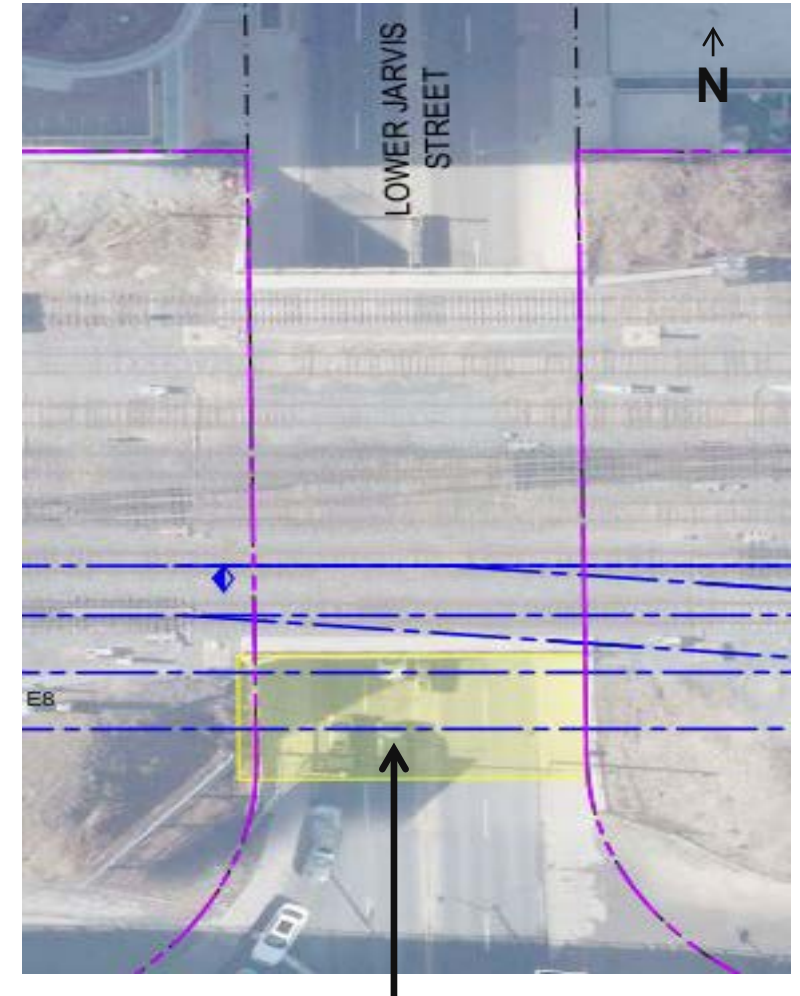
Track E0

BRIDGE EXTENSION – LOWER JARVIS (SOUTH SIDE)

- The rail bridge will be extended about 11 metres to the south to add two new tracks – E7 & E8
- 2 new retaining walls
- Road work and lane restrictions will be required to build the bridge extension



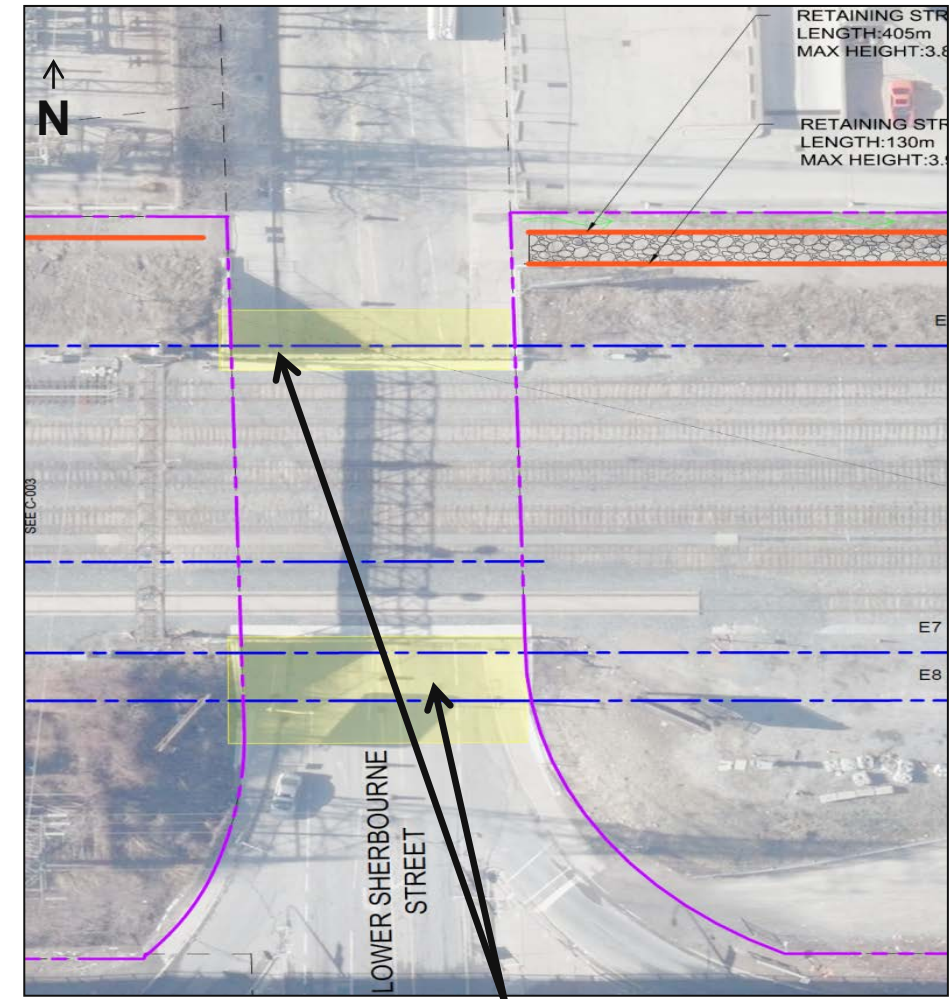
South



Bridge Extension for
Tracks E7 & E8

BRIDGE EXTENSION – LOWER SHERBOURNE

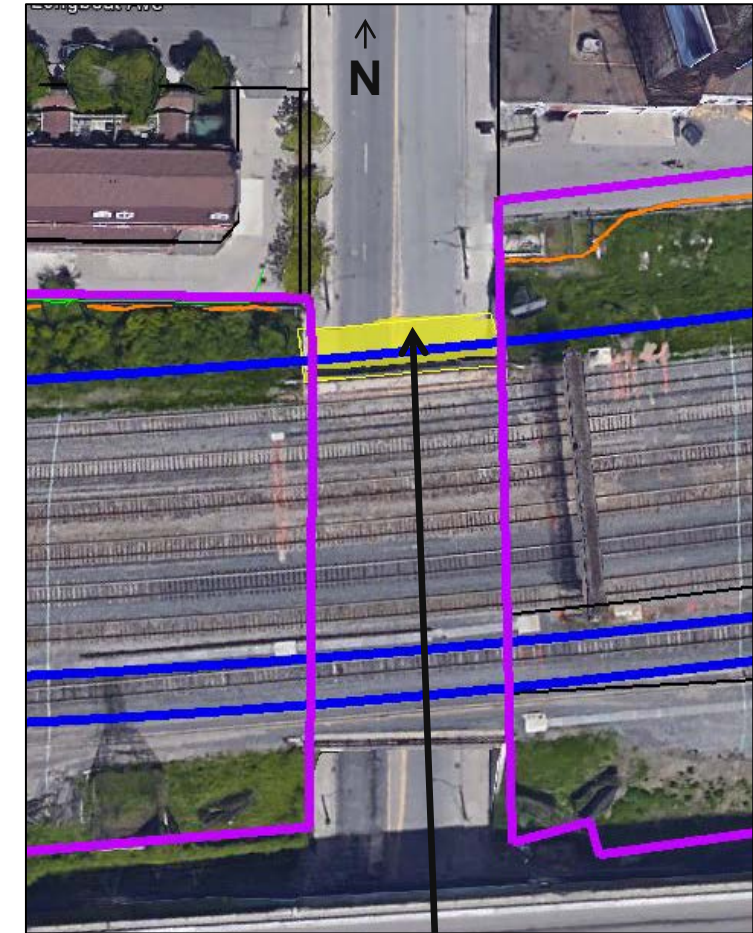
- The rail bridge will be extended about 7 metres to the north to extend track E0
- It will be extended about 11 metres to the south for tracks E7 & E8
- 2 new wing walls
- Road work and lane restrictions will be required for bridge extension



**Bridge Extension for
Tracks E0, E7 & E8**

BRIDGE EXTENSION – PARLIAMENT (NORTH SIDE)

- The rail bridge will be extended about 6 metres to the north to extend track E0
- New retaining wall
- Road work and lane restrictions will be required to build the bridge extension



**Bridge Extension
for Track E0**

BRIDGE EXTENSION – CHERRY (NORTH SIDE)

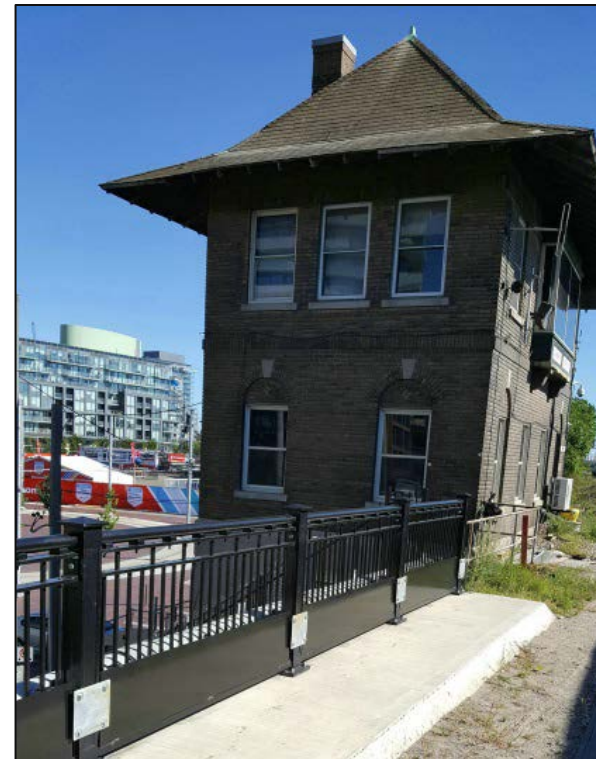
- The rail bridge will be extended about 5 to 7 metres to the north to extend track E0
- New retaining wall
- Road work and lane restrictions will be required to build the bridge extension



**Bridge Extension
for Track E0**

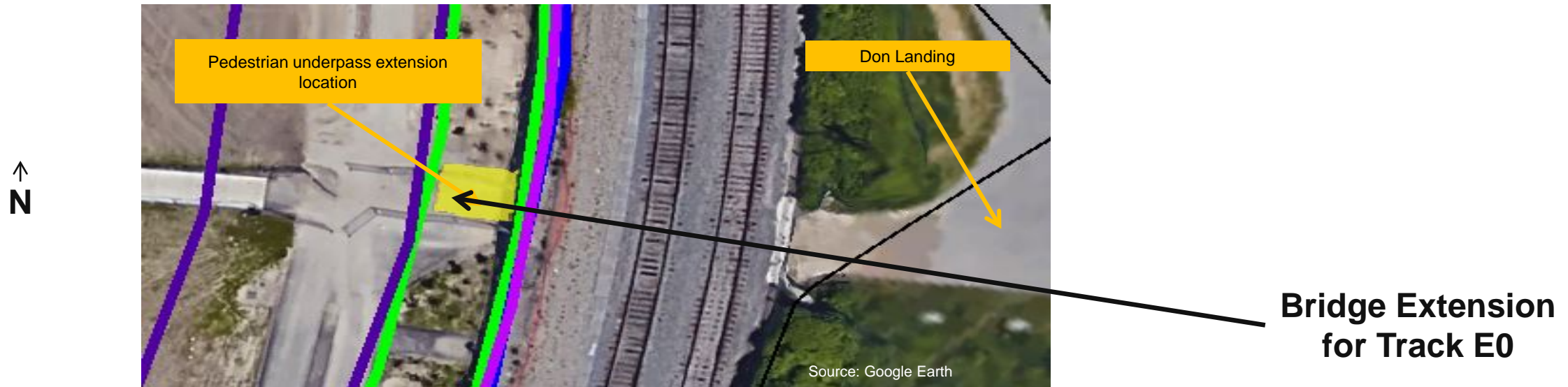
POSSIBLE CHERRY STREET TOWER RELOCATION

- The current location of the Cherry Street Tower is in the path of the extension of Track E0
- Reviewing options including relocation of the tower to accommodate track extension



BRIDGE EXTENSION – LOWER DON RIVER TRAIL

- The rail bridge will be extended about 5 to 6 metres to the west to extend track E0
- There will be some temporary impacts to the trail underpass access



TRACK STORAGE – WILSON YARD

- Add up to 5 more train storage tracks to the existing 3 tracks
- Need more train storage with increase in GO service
- Need a total of up to 18 storage tracks in this area to support future GO and SmartTrack service

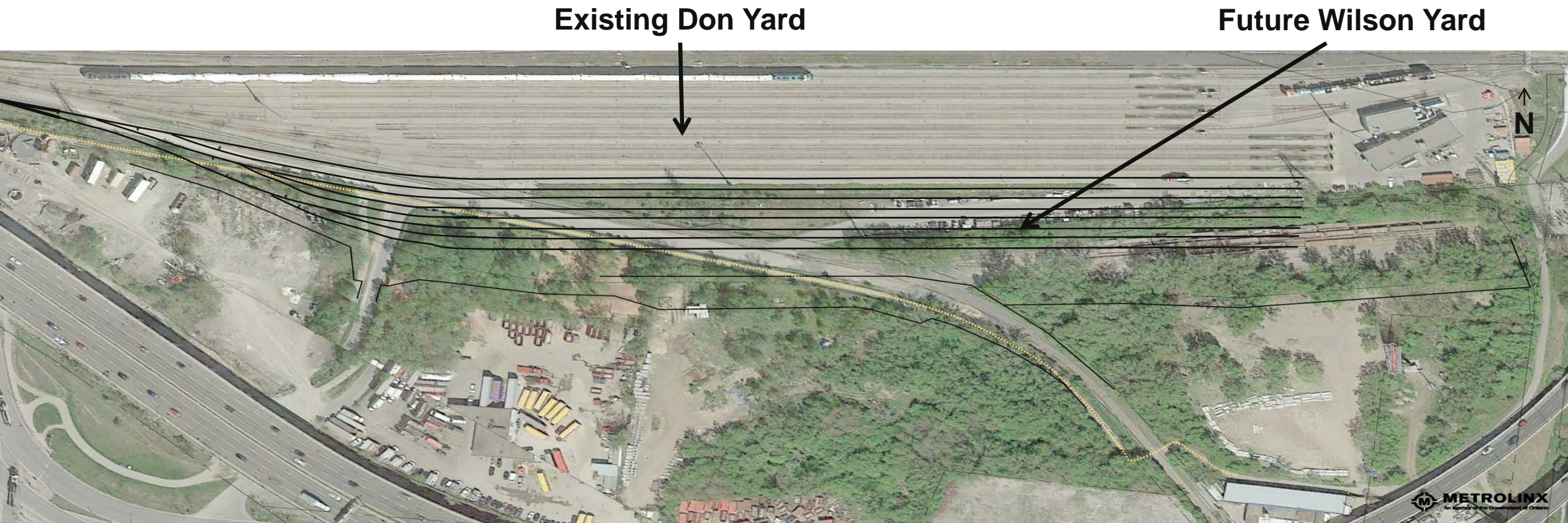


Existing Don Yard

Future Wilson Yard

TRACK STORAGE – WILSON YARD (cont'd)

- Up to 8 tracks to be built south of the Don Yard



WHY WE NEED NEW TRACKS

- **More Service** – GO service expansion will double the number of trips in rush and quadruple service outside of rush hour
- **More Capacity** – To deal with this transformational increase in service, new tracks will increase the service capacity in the Union Station Rail Corridor
- **Faster Service** – It will also speed up service by reducing the number of switches to transfer trains between tracks leaving Union Station

CO-ORDINATION WITH OTHER PROJECTS

- **Keating Channel Precinct Plans (East and West)**
- **Gardiner Expressway and Lake Shore Boulevard East Reconfiguration EA**
- **Gardiner East Urban Design Study and Public Realm Implementation Plan**
- **Don Mouth Naturalization and Port Lands Flood Protection Project EA**
- **Don River & Central Waterfront Class EA**
- **Lower Don Lands Infrastructure Master Plan EA**
- **Lower Don Master Plan**
- **New Harbour East GO / SmartTrack Station**
- **Unilever Site Development**

COMMUNITY CONCERNS

- **Visual Impacts** – removal of trees/vegetation
- **Increased Noise** – more GO service
- **Connections to the Waterfront** – concern about restrictions to access to the waterfront
- **Overnight Work** – concern about extended duration of overnight work to add new tracks and train storage
- **Vandalism/Graffiti** – new infrastructure (e.g. retaining walls) have the potential to become a canvas for graffiti

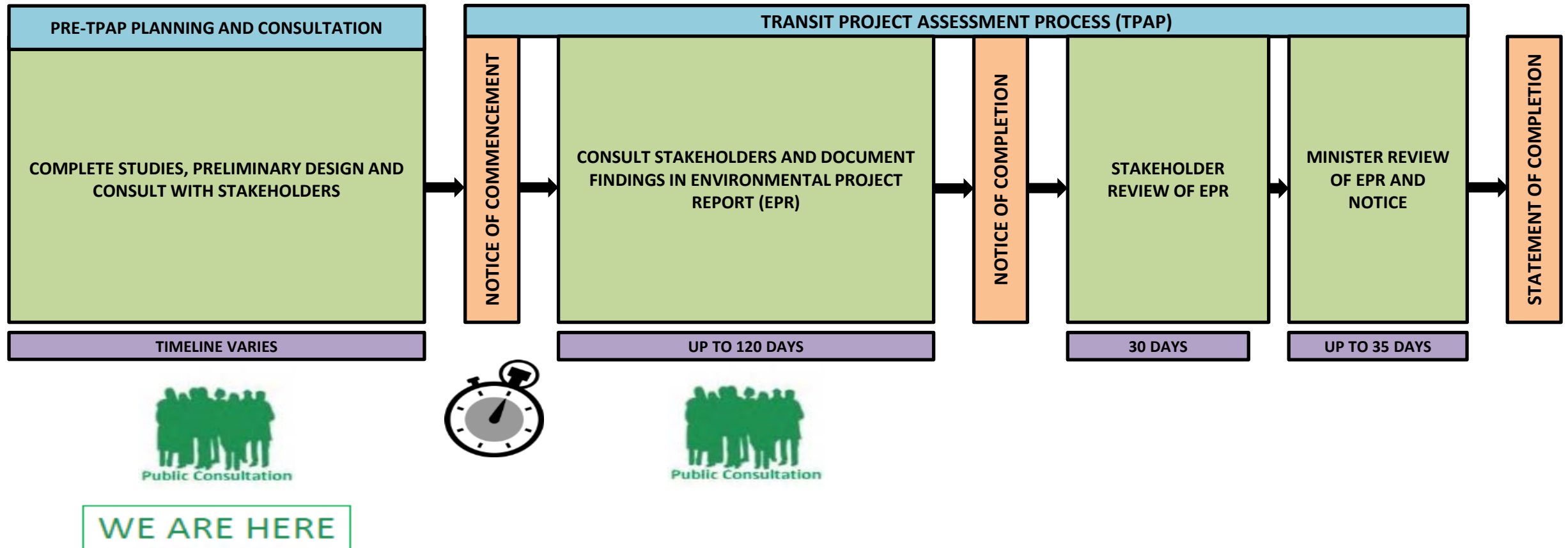
WHERE WE ARE AT

- Engaging with stakeholders (e.g. City of Toronto, community groups) and regulators (e.g. Ministry of the Environment and Climate Change)
- Working on environmental studies to assess impacts and benefits from our plans
- Completing preliminary design
- Co-ordinating with City of Toronto, Waterfront Toronto, and Toronto and Region Conservation Authority staff

TRANSIT PROJECT ASSESSMENT PROCESS

- A TPAP is a streamlined environmental assessment process to expedite the development of transit projects.
- TPAP steps:
 - Understand existing conditions
 - Review other options considered
 - Describe the preferred option
 - Complete conceptual design of preferred option
 - Assess impacts of preferred option
 - Undertake stakeholder engagement and public consultation
 - Determine mitigation measures for any negative project impacts
 - Prepare Environmental Project Report (EPR)

TRANSIT PROJECT ASSESSMENT PROCESS



NEXT STEPS

- **Community Engagement** – May/June
- **Public Meeting #1** – Late June [TBC]
- **Notice of Commencement** – Early Fall [TBC]
- **Public Meeting #2** - Early Fall [TBC]

For more information:

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