



# UNION STATION EAST ENHANCEMENTS

# GRIDLOCK COSTS OUR ECONOMY

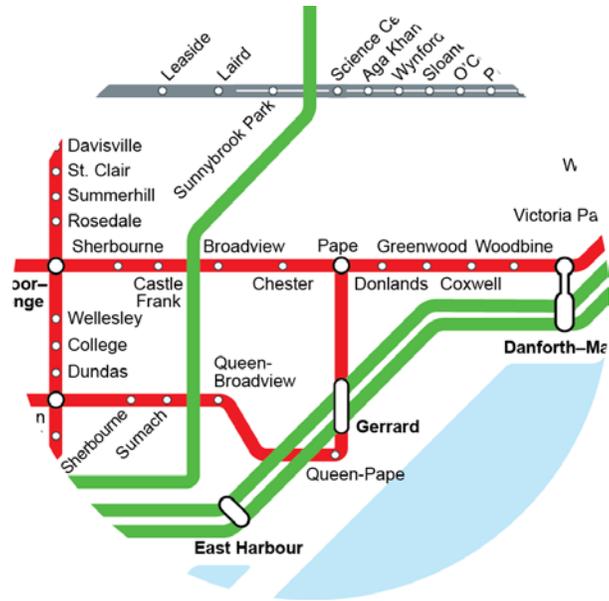


# REGIONAL TRANSFORMATION



# METROLINX

## PLAN



## BUILD



## OPERATE



# GO EXPANSION PROGRAM



# INFRASTRUCTURE NEEDS FOR GO EXPANSION

- Bringing more transit options and new levels of service doesn't come easy.
- Metrolinx is undertaking one of the largest infrastructure projects in North America.
- There is community-level disruption required to build a region-wide, integrated transit network.



## New Track

150 kilometres of new dedicated GO track will allow for more uninterrupted service.



## New Trains

New electric trains will travel faster for longer and reduce travel times.



## New Bridges and Tunnels

Bridges and tunnels that eliminate intersections with rail and road traffic will provide more reliable GO train service.



## New Renovations

New and improved stations will make your journey more comfortable, from start to finish.



## Overhead Catenary

# WHAT THIS MEANS FOR YOU

In order to bring more GO for you and your neighbours in the Greater Toronto and Hamilton Area, Metrolinx needs to do the following:

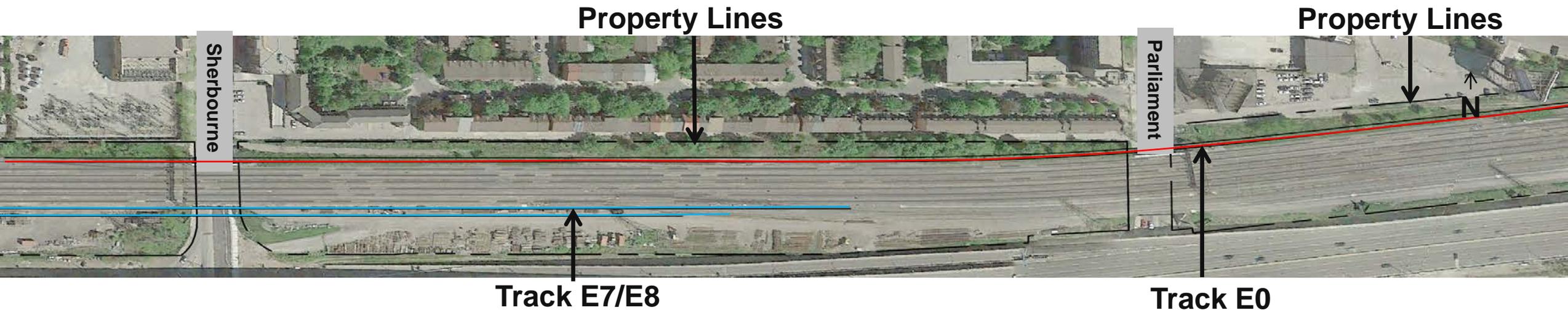
- Add tracks
- Bridge extensions to accommodate new tracks
- Add train storage capacity

# ADD TRACK - BETWEEN JARVIS & SHERBOURNE

- Track E0 – to the north; existing track to be extended to the east.
- Tracks E7 & E8 – to the south

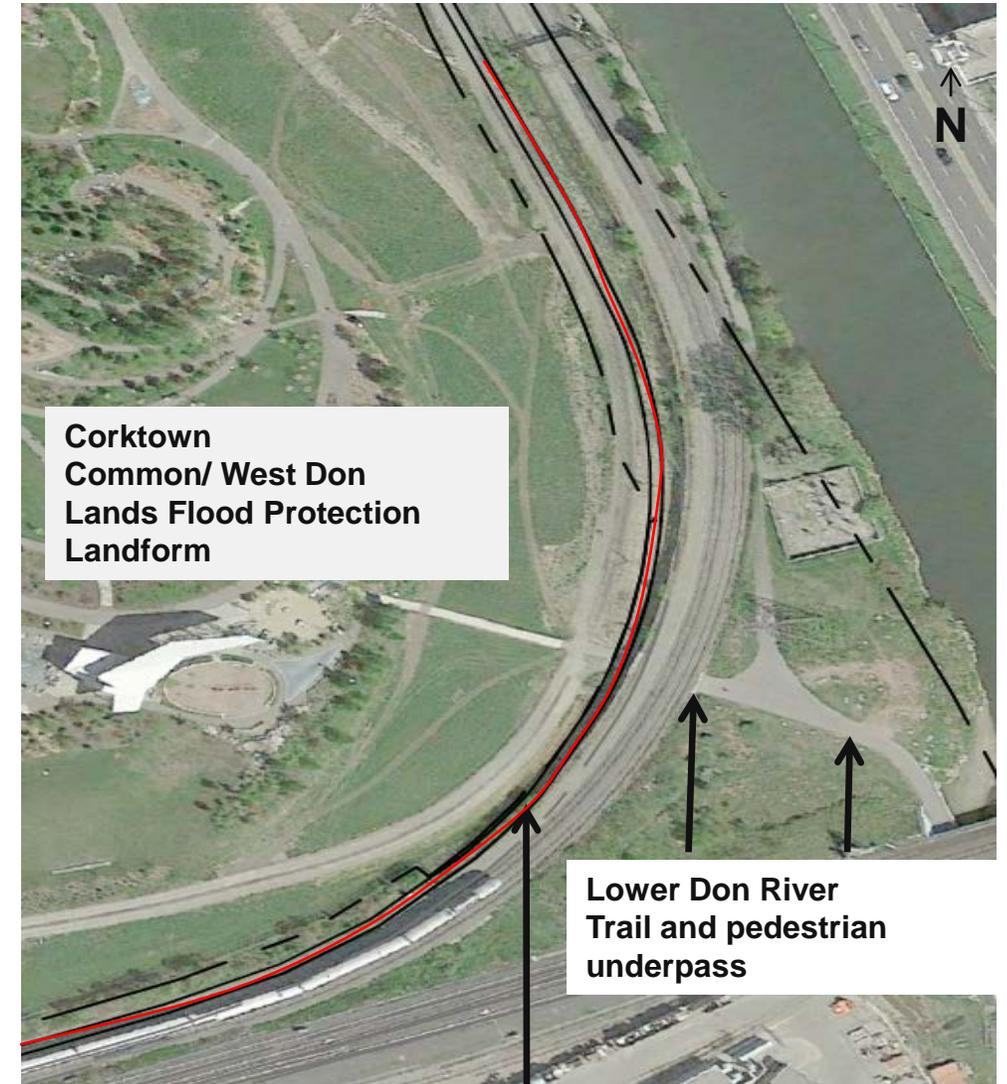


# ADD TRACK – BETWEEN SHERBOURNE & DON RIVER



# ADD TRACK – CORKTOWN COMMON/WEST DONLANDS

- Majority of this section within the Don River floodplain
- Lower Don River Trail pedestrian underpass will require widening
- Trail disruptions during construction at pedestrian underpass and within Corktown Common



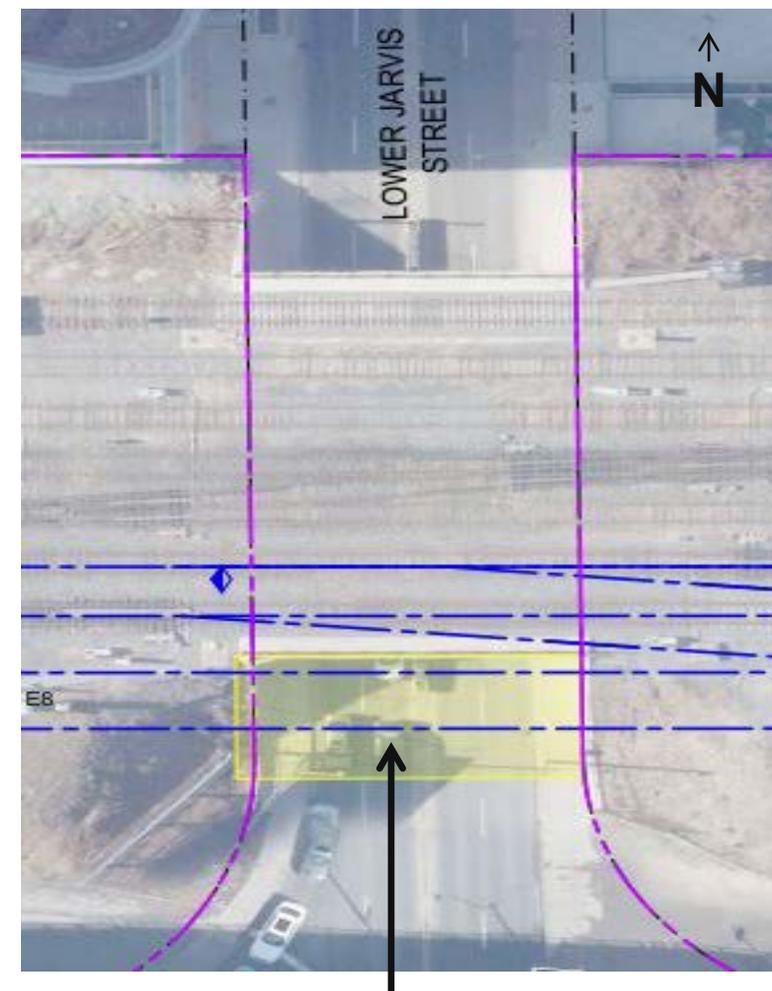
Track E0

# BRIDGE EXTENSION – LOWER JARVIS (SOUTH SIDE)

- The rail bridge will be extended about 11 metres to the south to add two new tracks – E7 & E8
- 2 new retaining walls
- Road work and lane restrictions will be required to build the bridge extension



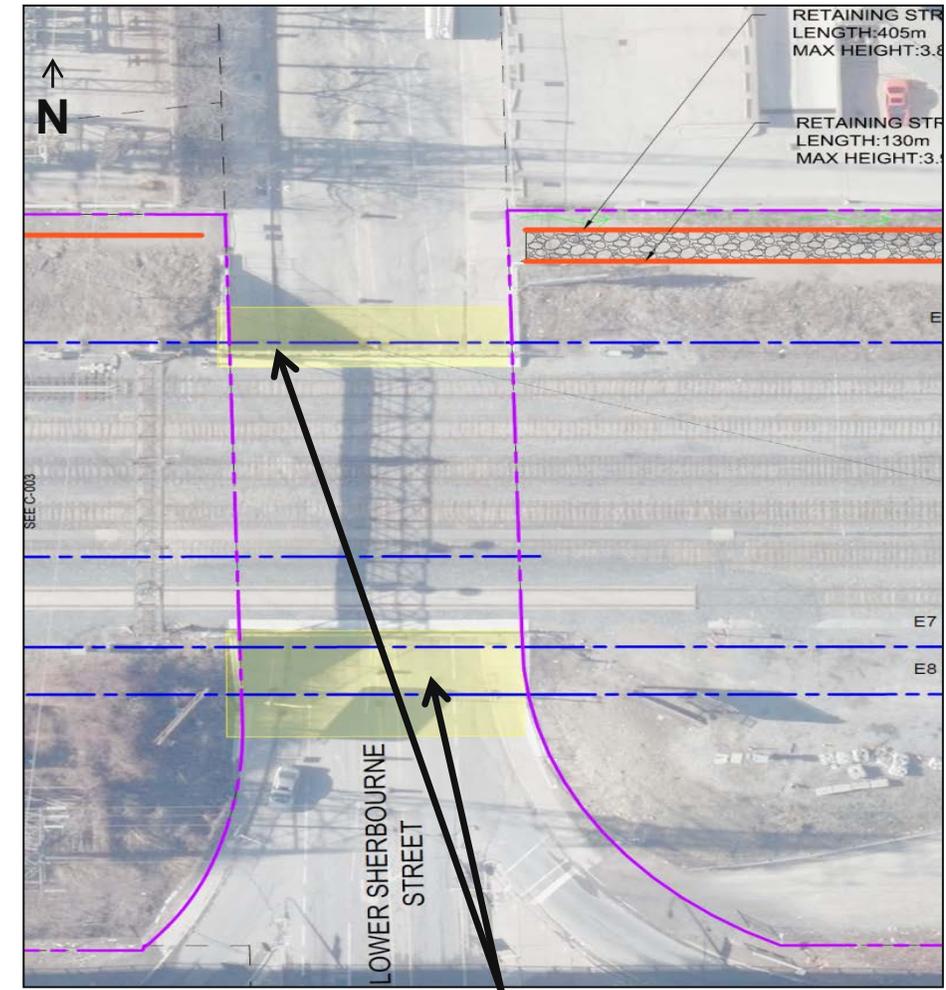
South



Bridge Extension for  
Tracks E7 & E8

# BRIDGE EXTENSION – LOWER SHERBOURNE

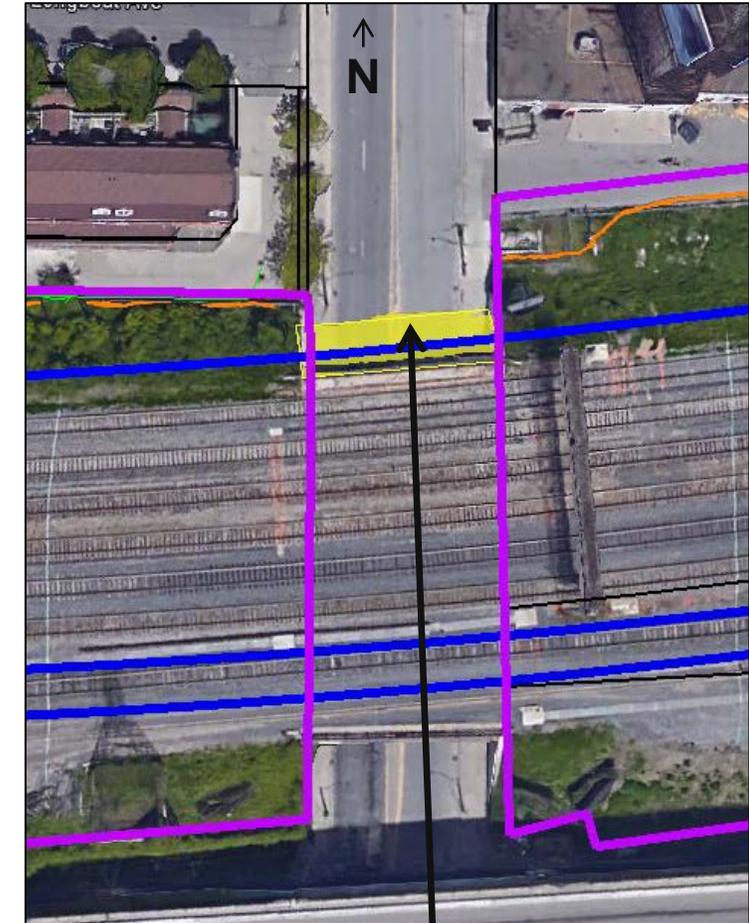
- The rail bridge will be extended about 7 metres to the north to extend track E0
- It will be extended about 11 metres to the south for tracks E7 & E8
- 2 new wing walls
- Road work and lane restrictions will be required for bridge extension



**Bridge Extension for  
Tracks E0, E7 & E8**

# BRIDGE EXTENSION – PARLIAMENT (NORTH SIDE)

- The rail bridge will be extended about 6 metres to the north to extend track E0
- New retaining wall
- Road work and lane restrictions will be required to build the bridge extension



**Bridge Extension  
for Track E0**

# BRIDGE EXTENSION – CHERRY (NORTH SIDE)

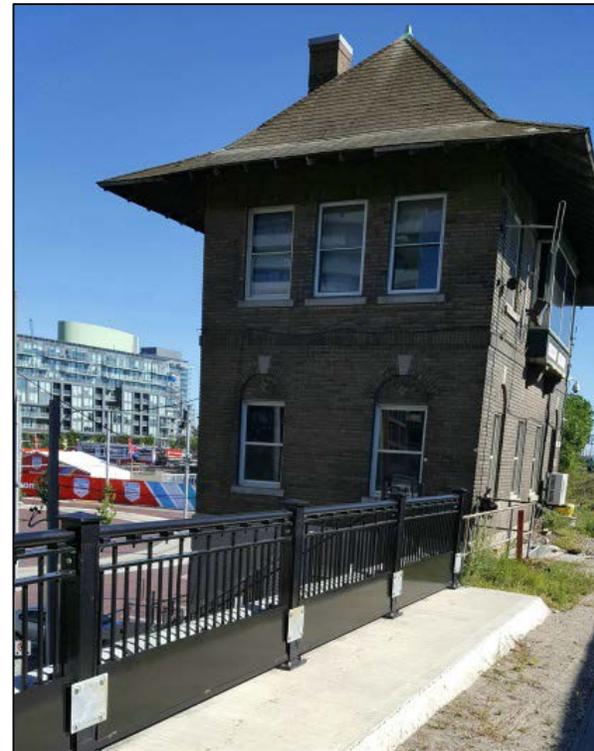
- The rail bridge will be extended about 5 to 7 metres to the north to extend track E0
- New retaining wall
- Road work and lane restrictions will be required to build the bridge extension



**Bridge Extension  
for Track E0**

# POSSIBLE CHERRY STREET TOWER RELOCATION

- The current location of the Cherry Street Tower is in the path of the extension of Track E0
- Reviewing options including relocation of the tower to accommodate track extension



# BRIDGE EXTENSION – LOWER DON RIVER TRAIL

- The rail bridge will be extended about 5 to 6 metres to the west to extend track E0
- There will be some temporary impacts to the trail underpass access



**Bridge Extension  
for Track E0**

# TRACK STORAGE – WILSON YARD

- Add up to 5 more train storage tracks to the existing 3 tracks
- Need more train storage with increase in GO service
- Need a total of up to 18 storage tracks in this area to support future GO and SmartTrack service

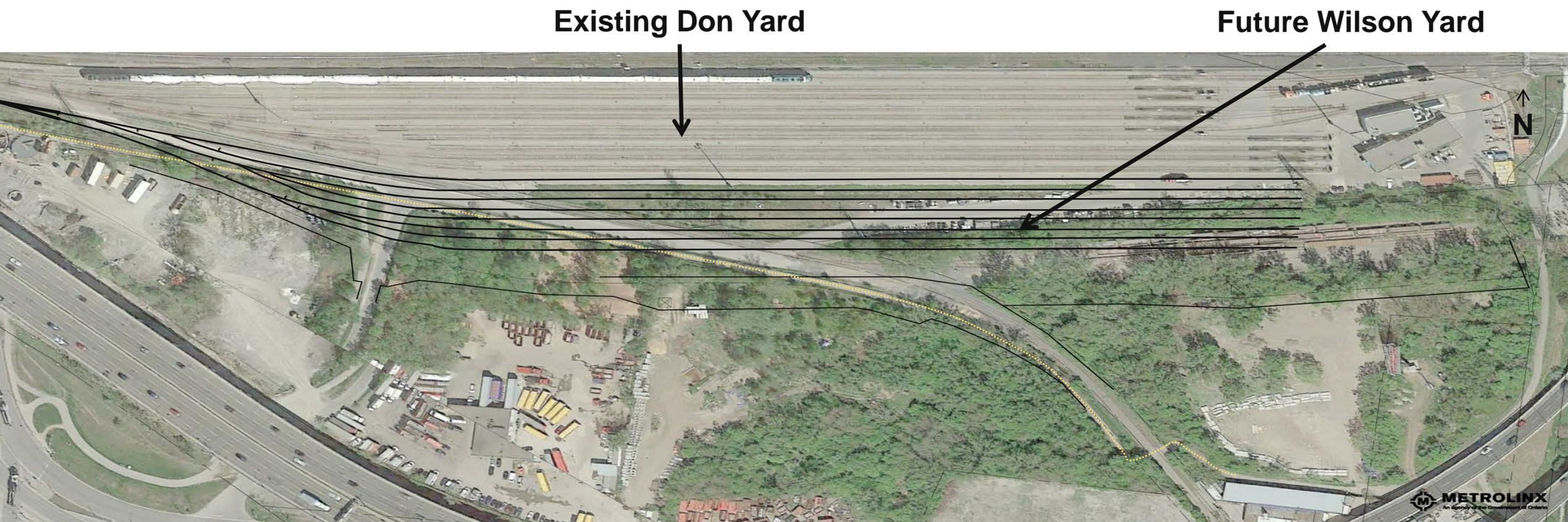


Existing Don Yard

Future Wilson Yard

# TRACK STORAGE – WILSON YARD (cont'd)

- Up to 8 tracks to be built south of the Don Yard



# WHY WE NEED NEW TRACKS

- **More Service** – GO service expansion will double the number of trips in rush and quadruple service outside of rush hour
- **More Capacity** – To deal with this transformational increase in service, new tracks will increase the service capacity in the Union Station Rail Corridor
- **Faster Service** – It will also speed up service by reducing the number of switches to transfer trains between tracks leaving Union Station

# CO-ORDINATION WITH OTHER PROJECTS

- **Keating Channel Precinct Plans (East and West)**
- **Gardiner Expressway and Lake Shore Boulevard East Reconfiguration EA**
- **Gardiner East Urban Design Study and Public Realm Implementation Plan**
- **Don Mouth Naturalization and Port Lands Flood Protection Project EA**
- **Don River & Central Waterfront Class EA**
- **Lower Don Lands Infrastructure Master Plan EA**
- **Lower Don Master Plan**
- **New Harbour East GO / SmartTrack Station**
- **Unilever Site Development**

# COMMUNITY CONCERNS

- **Visual Impacts** – removal of trees/vegetation
- **Increased Noise** – more GO service
- **Connections to the Waterfront** – concern about restrictions to access to the waterfront
- **Overnight Work** – concern about extended duration of overnight work to add new tracks and train storage
- **Vandalism/Graffiti** – new infrastructure (e.g. retaining walls) have the potential to become a canvas for graffiti

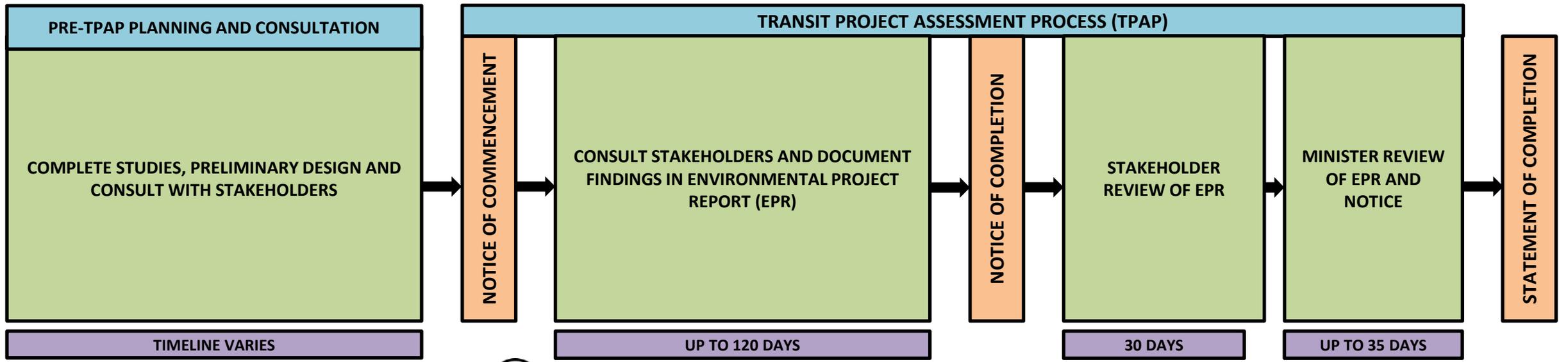
# WHERE WE ARE AT

- Engaging with stakeholders (e.g. City of Toronto, community groups) and regulators (e.g. Ministry of the Environment and Climate Change)
- Working on environmental studies to assess impacts and benefits from our plans
- Completing preliminary design
- Co-ordinating with City of Toronto, Waterfront Toronto, and Toronto and Region Conservation Authority staff

# TRANSIT PROJECT ASSESSMENT PROCESS

- A TPAP is a streamlined environmental assessment process to expedite the development of transit projects.
- TPAP steps:
  - Understand existing conditions
  - Review other options considered
  - Describe the preferred option
  - Complete conceptual design of preferred option
  - Assess impacts of preferred option
  - Undertake stakeholder engagement and public consultation
  - Determine mitigation measures for any negative project impacts
  - Prepare Environmental Project Report (EPR)

# TRANSIT PROJECT ASSESSMENT PROCESS



WE ARE HERE

# NEXT STEPS

- **Community Engagement** – May/June
- **Public Meeting #1** – Late June [TBC]
- **Notice of Commencement** – Early Fall [TBC]
- **Public Meeting #2** - Early Fall [TBC]

For more information:

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